

City of Littleton

LITTLETON BOULEVARD CORRIDOR STUDY

DESIGN GUIDELINES FOR:

- **THE SHERIFF'S DEPARTMENT SITE**
(2000 W. Littleton Boulevard)
- **THE COMMUNITY CENTER SITE**
(1900 W. Littleton Boulevard)

**Adopted by Planning Commission
January 14, 2002**

**Ratified by City Council
February 19, 2002**

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PURPOSE AND GOALS

PURPOSE

Littleton Boulevard historically was the main commercial street in Littleton. With the heavy development on Broadway and the redevelopment of shopping centers to the west of downtown, Littleton Boulevard has become a street with local services, auto related uses and neglected shopping centers. The City of Littleton has improved the pedestrian walks along the street, allowing it to be a friendlier street for pedestrian use.

Littleton Boulevard is a 'seam' between the neighborhoods to the north and south, and is a major arterial for traffic moving east and west.

It is also the 'spine' which generates and connects street/pedestrian/bike connections to downtown, the Littleton Community Center, the Historic Courthouse, Sterne and Ketring Park to the south, and Powers Park to the north.

Littleton Boulevard also serves as a major east/west connector for commuters and city residents. Littleton Boulevard pairs with either Belleview Avenue to the north or Orchard Road to the south even though it does not connect through to the east past Broadway. While this helps to reduce the overall through traffic, because of the relatively few east/west connectors in the south metro area, Littleton Boulevard will always have an important role. It currently carries approximately 24,000 cars per day.

Future uses should take advantage of the easy connection to light rail and the centralized location of the corridor.



GOAL

- Establish Littleton Boulevard as one of the great streets in the Denver metropolitan area.

OBJECTIVES

- Identify and strengthen the role of the street as a multi-modal connection with the City.
- Develop a street that fits the City of Littleton image and character.
- Develop Guidelines that respond to a mix of uses along the corridor.
- Identify and develop appropriate guidelines for focal points along the corridor that supports connections to adjacent neighborhoods and parks.
- Identify and develop appropriate guidelines for gateway opportunities into and from downtown Littleton.
- Preserve and strengthen the historic identity of the adjacent residential neighborhoods.
- Improve pedestrian and bicycle circulation and safety.

SITE CONTEXT

The Sheriff's Department is located due east of the railroad depression and fronts Littleton Boulevard and Bemis Streets. The detention basin that feeds Little's Creek is directly south of the site. The 2.98 acre site is currently zoned B-2.

The Community Center is one block east of the Sheriff's Department. It is bound by Littleton Boulevard on the north, Spotswood Street to the west, Prescott Street to the east, and Lilley Avenue to the south. The 1.74 acre site is currently zoned Transitional.



Littleton Boulevard Corridor Plan (west half)

Sheriff's Department and Community Center Site Context



Existing Sheriff's Department and Community Center Sites



Littleton Boulevard Corridor Plan (east half)

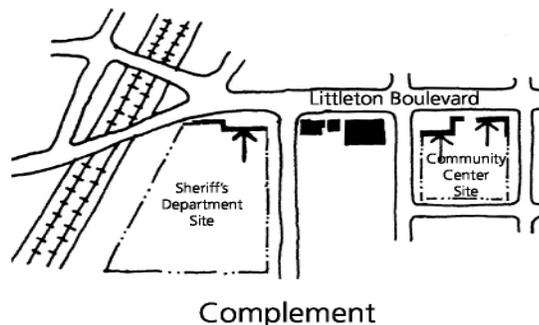
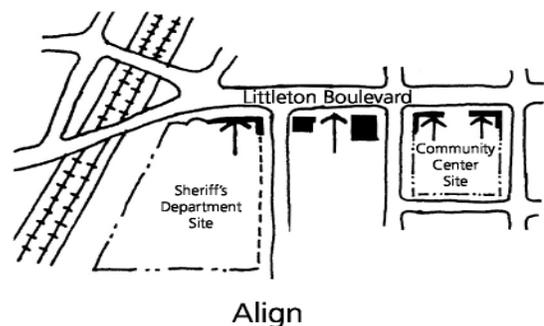
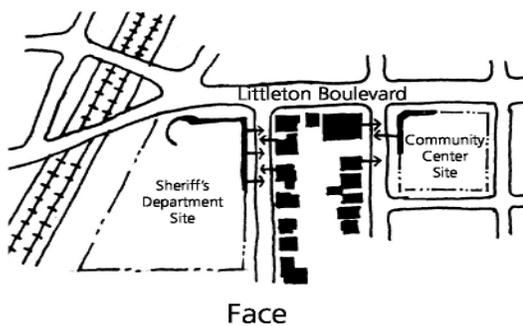
DESIGN GUIDELINES COMMON TO BOTH THE SHERIFF'S DEPARTMENT AND COMMUNITY CENTER SITES

1A: SITE PLAN/URBAN DESIGN

1A1: BUILDING RELATIONSHIPS

- Goal 1:** *To fit with the character of Littleton Boulevard in this area.*
- Goal 2:** *To promote the development of buildings that encourages pedestrian activity through the incorporation of pedestrian-oriented uses at the ground level street frontage.*
- Goal 3:** *To fit with the existing neighborhood.*

- 1A1.1:** Buildings should be oriented toward the street.
- 1A1.2:** Buildings should be generally set back from the street and generally aligned with each other. Variation in the set back and/or alignment may be appropriate at the discretion of the Development Review Committee.
- 1A1.3:** Buildings should align with, face, or otherwise complement adjacent single family residences and historic houses.



1A2: VEHICULAR ACCESS AND PARKING

Goal: *To minimize the view of parking from streets, public spaces, and facilities.*

- 1A2.1:** Avoid locating parking between a building's frontage and a street or open space. Surface parking lots or structured parking should be located at the side or rear of buildings, with the understanding that some or all of a few less prominent street frontages or exposed property lines may be bordered by parking areas.
- 1A2.2:** The width of driveways and curb cuts should be minimized as much as possible to reduce the overall impact of vehicular access across a sidewalk.
- 1A2.3:** Curb cuts should not occur on Littleton Boulevard. Driveways should be located on the side streets leading to Littleton Boulevard.

1A3: PEDESTRIAN CIRCULATION AND ACCESS

Goal: *To provide pedestrian circulation continuity within and between developments, to the transit system, and to downtown.*

- 1A3.1:** Ground floor uses with exterior exposures should each have a public entry directly connected to a public sidewalk along a street, or on a sidewalk or plaza leading directly to a sidewalk along a street.
- 1A3.2:** Primary building entrances should be oriented towards streets or pedestrian plazas.

1B: LANDSCAPE ARCHITECTURE

1B1: LANDSCAPING IN RIGHT-OF-WAY

Goal 1: *To create a continuous streetscape character using a limited palette of tree species.*

Goal 2: *To create an attractive, safe, and comfortable environment for pedestrians adjacent to the street.*

1B1.1: The existing trees and tree lawns should be preserved when new development occurs.

1B1.2: Sidewalks on side streets should be detached and trees should be located no closer than 30 inches from the back of curb and 24 inches from the detached sidewalk.

1B1.3: Tree species should be consistent with the existing mature species or as specified in the Landscape Manual referenced in the City of Littleton Code 10-5-1 C.2.

1B1.4: Street trees should not encroach upon existing and planned utility lines or easements.

1B2: LANDSCAPING IN FRONT SETBACK

Goal: *To integrate planting schemes with the architecture.*

1B2.1: Plantings should be placed to complement the architecture. It should not be conceived as a screen for poorly designed buildings.

1B2.2: Plantings should be used to create space associated with architecture, not just as foreground plantings.

1B2.3: Plantings should be located in a manner that encourages outdoor café seating and window shopping where ground floor retail occurs.

1B3: INTERIOR OPEN SPACE

Goal: *To create useable open space, suitable for passive recreational activities such as informal play, reading, and sitting in the sun.*

1B3.1: Where site area allows, interior courtyards and plazas should be encouraged.

1B4: EXISTING LANDSCAPING

Goal: *To incorporate existing healthy mature trees and shrubs into the landscape design.*

1B4.1: Existing trees and other plants in reasonably healthy condition should be incorporated into the landscape design.

1B5: PARKING LOT LANDSCAPING

Goal: *To reduce the scale of surface parking lots and to visually separate parking lots from adjoining streets.*

1B5.1: For internal parking lot landscaping, deciduous tree species and evergreen or deciduous shrub species should be used.

1B5.2: Landscaping for interior parking lot islands should include two trees per island and a continuous shrub or groundcover. Sod in parking lot islands is not recommended.

1B5.3: As per City of Littleton landscaping code 10-5-7, landscaped parking lot islands must be delineated by a clear physical barrier such as concrete curbs or set landscaping timbers to protect the plant material from vehicular damage. In addition, a minimum of five percent (5%) of the interior area of a parking lot shall be landscaped if the lot contains fifteen (15) or more spaces.

1B6: PEDESTRIAN / BIKE FACILITIES

Goal: *To encourage the use of bicycles as a means of access to the new development.*

1B6.1: Safe, conveniently located, and adequate bicycle parking should be provided.

1B6.2: Bicycle parking should be located as close to building entrances as possible.

1B7: PLAZAS

Goal: *To create active, inviting, and comfortable public environments.*

1B7.1: Plazas, courtyards, or other usable open spaces should be made comfortable by providing elements such as benches, low walls suitable for seating, trees, shade structures, fountains, lighting, and trash receptacles.

1C: ARCHITECTURE

1C1: MASSING / HEIGHT / SCALE

Goal: *To moderate scale changes between adjacent lots.*

- 1C1.1:** Taller buildings should step down to lower buildings, avoiding abrupt changes in height. Other facade treatments such as approximate alignments of windows, storefront heights, and belt course can also help to create more continuity between buildings.
- 1C1.2:** Building form next to Littleton Boulevard and Bemis Street should be a maximum of three stories, stepping up in height away from the street.

1C2: ORIENTATION TO STREET. VIEWS, OPEN SPACE, NEIGHBORHOOD

Goal: *To create visually interesting facades, particularly those that face streets.*

1C3: ARCHITECTURAL CHARACTER

- Goal 1:** *The architectural character should complement the existing historic architecture of this subarea.*
- Goal 2:** *To create buildings that provide human scale, interest and variation.*

- 1C3.1:** All buildings should be designed specially for the context and character of the area. Unmodified corporate standard building design should not be allowed.
- 1C3.2:** The buildings should possess an architectural character that reinforces the historic character of the existing buildings in this area.

1C4: MATERIALS / COLORS

- Goal 1:** *To relate new construction to that of existing construction through the use of similar scale elements.*
- Goal 2:** *To use materials that convey a sense of quality, permanence, and care.*

- 1C4.1:** The material choices and their use should be durable, permanent, and express quality.
- 1C4.2:** Materials and forms that cast shadows, catch light, and take advantage of Colorado's sunny climate should be used.

1C5: BUILDING LIGHTING

Goal: *To insure a level of quality in the design of light fixtures appropriate to their location, prominence, and roles.*

1C5.1: Exterior building lighting is encouraged and should accentuate building elements such as entrances, bays corner elements, and structure

1C5.2: Light pollution/spill light to adjacent residential uses should be minimized.

1C6: ROOFTOP

Goal: *Rooftops should relate to the existing character of this historic sub-area along Littleton Boulevard.*

1C6.1: Sloped roofs are preferred over flat roofs.

1C6.2: Rooftop mechanical equipment units should be appropriately screened and/or integrated into the form of the roof or building.

1C7: PARKING GARAGE FAÇADE (IF ANY)

Goal 1: *To minimize the visual impact of parking on the pedestrian experience, and the street environment.*

Goal 2: *To design the garage facades visible from public streets and open spaces so that they are compatible in character and quality with adjoining buildings.*

1C7.1: Wrapping the exterior of parking garages with buildings is highly encouraged.

1C7.2: If it is only feasible to orient the long dimension of a garage along a street, then the garage's street facade shall exhibit the same high level of quality in its design, detailing and use of material as is provided in the adjoining buildings.

DESIGN GUIDELINES UNIQUE TO THE SHERIFF'S DEPARTMENT SITE

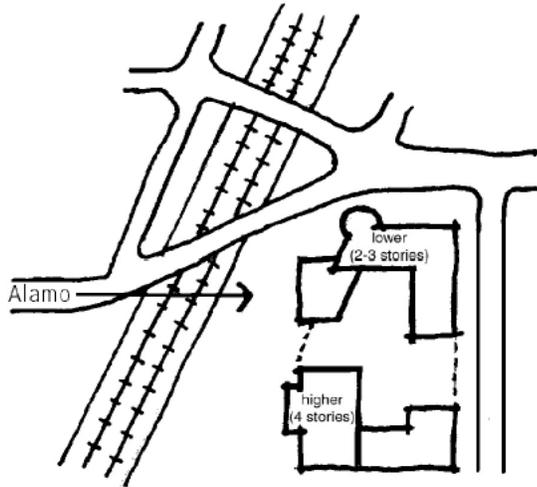
2A: SITE PLAN/URBAN DESIGN

2A1: BUILDING RELATIONSHIPS

Goal 1: To terminate the view from Alamo Avenue looking east in an attractive way.

Goal 2: To present an attractive west façade to the light rail station and downtown.

2A1.1: A building or a portion of a building should be located on an axis with Alamo



Avenue and designed to present an attractive and interesting facade oriented to Alamo Avenue.

2A1.2: Building frontages should be provided along all streets, open space and railway frontages. Continuous building frontages are strongly preferred along Littleton Boulevard and the open space to the south. Intermittent frontages are acceptable along Bemis Street and the railroad line. Building mass is preferred at the intersection of Littleton Boulevard and the railroad line to emphasize a gateway to the Littleton Boulevard Corridor.

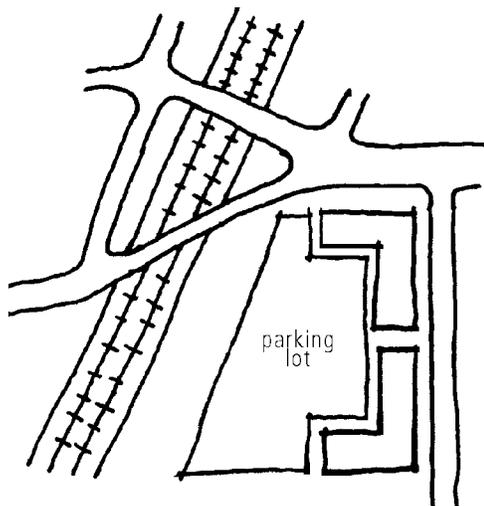


- 2A1.3:** Garage entries, service and loading docks should not be located at the terminus of the Alamo Avenue view, nor along Littleton Boulevard or Bemis Street.
- 2A1.4:** A minimum of a ten (10) foot landscaped setback should be provided along the railroad frontage.

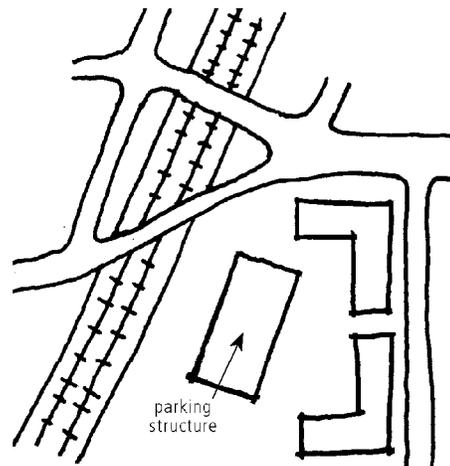
2A2: VEHICULAR ACCESS AND PARKING

Goal: *To minimize the view of parking from Littleton Boulevard, Alamo Avenue, and the downtown light rail stop.*

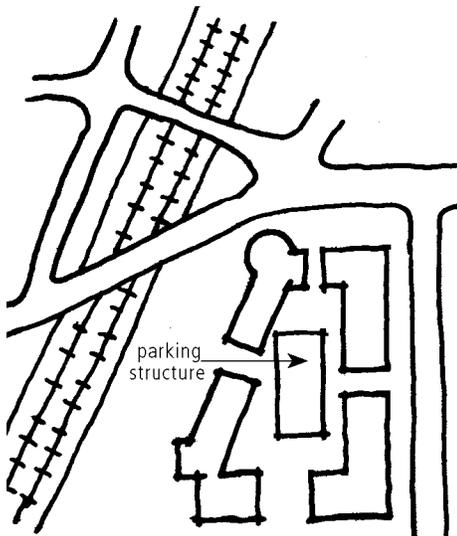
- 2A2.1:** At grade parking lots and/or above grade parking structures should not extend along the entire length or majority of the site boundary that faces the light rail stop.
- 2A2.2:** Parking lots or above grade parking structures should not extend along the Littleton Boulevard Frontage.



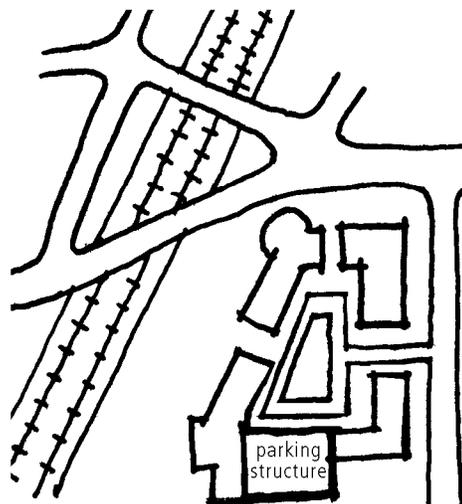
Not Desired



Not Desired



Desired



Desired

2A3: PEDESTRIAN CIRCULATION AND ACCESS

Goal: *To make easy and attractive pedestrian connections to the light rail stop and downtown.*

2A3.1: Pedestrian access from the site should directly connect to the Alamo Avenue Bridge across the railroad line and to the open space to the south.

2A3.2: Future access to a pedestrian bridge over the railroad depression and along the BN & SF railroad right-of-way should be provided.

2B: LANDSCAPE ARCHITECTURE

2B1: LANDSCAPING IN THE RIGHT-OF-WAY

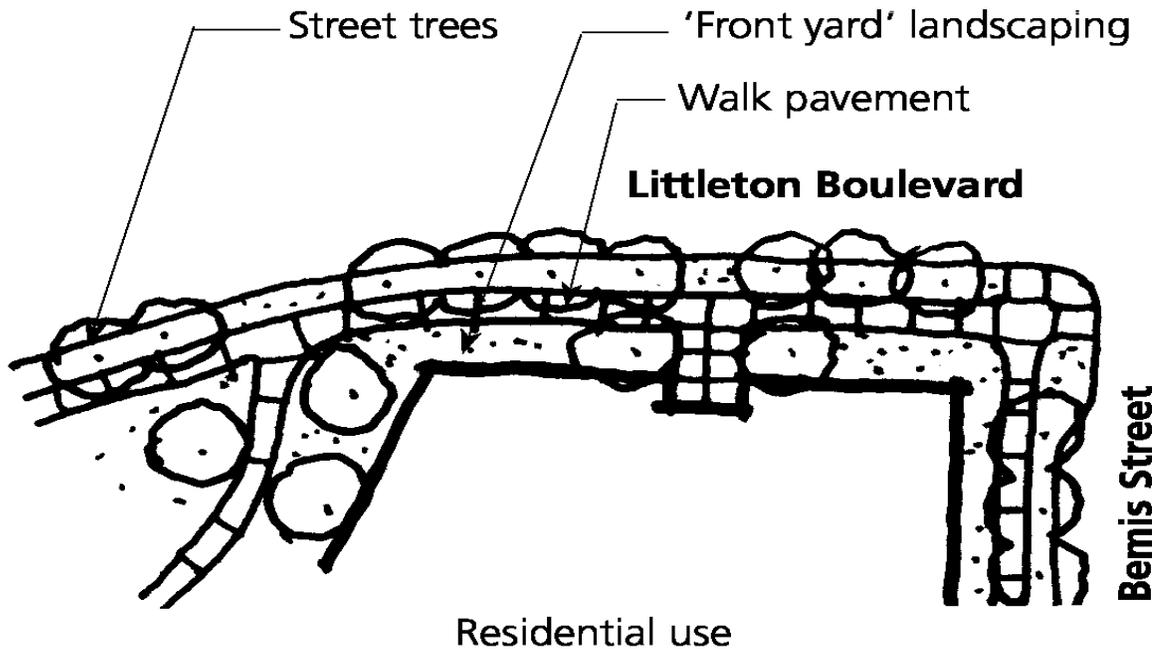
No additional goals or guidelines for this site.

2B2: LANDSCAPING IN THE FRONT SETBACK

Goal 1: *To reinforce a sense of entry into the Littleton Boulevard corridor environment.*

Goal 2: *To reflect the character of the landscape in the older, more historic area of the Boulevard.*

2B2.1: Extensive areas of hard surface should be mitigated by areas or panels of shrubs and/or turf with shade trees included if the setback space permits.

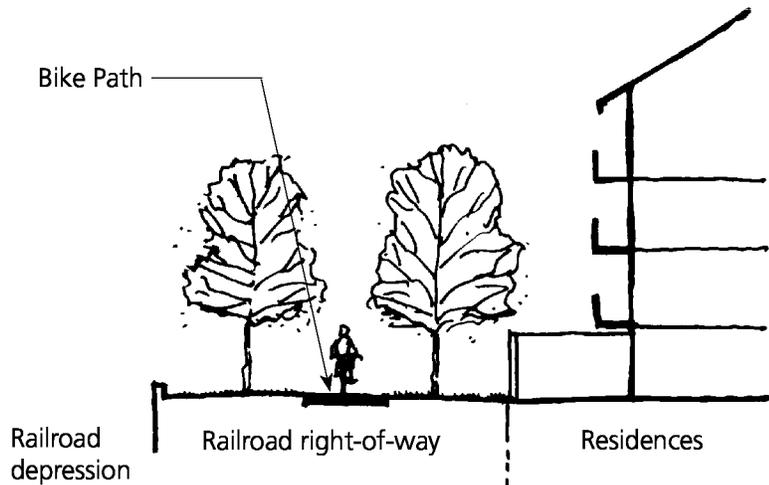


2B3: LANDSCAPING IN OTHER SETBACKS

Goal 1: *To buffer service oriented and support uses such as parking and loading docks.*

Goal 2: *To create an attractive edge to the light rail line and stop, to the open space south of the site, and to Bemis Street.*

- 2B3.1:** Where parking lots or above grade parking occur along the boundary with the railroad line, a continuous screen of trees and shrubs should be provided.
- 2B3.2:** Ground facades and/or private yards, decks, or fences that front onto the railroad property adjoining the railroad depression should orient toward this property and present high quality design and materials in anticipation of the railroad property being used as a bikeway.



- 2B3.3:** Opaque fencing or walls should be masonry or stucco. Transparent fencing should be metal picket or other forms of metal fencing. Typical chain link fencing is discouraged, as is typical wood board or stockade fencing. Vinyl fencing, either opaque or semi-transparent, may be acceptable depending on its character and detailing.

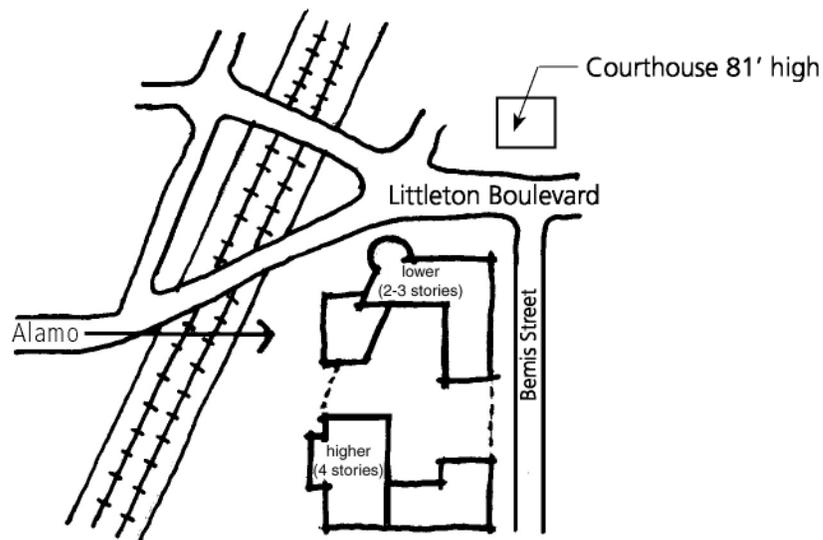
C: ARCHITECTURE

2C1: MASSING AND HEIGHT

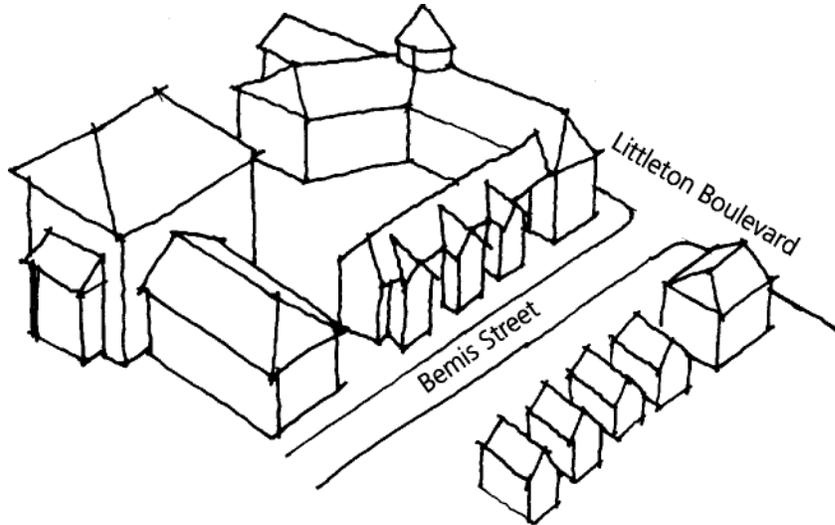
Goal 1: *To reflect the scale and height of the adjacent neighborhood and historic courthouse.*

Goal 2: *To maintain a two to three story edge to Littleton Boulevard, particularly along the south side to provide more sun exposure to the street.*

2C1.1: Higher building forms should be located at the southwest and west portions of the site stepping down toward Littleton Boulevard and Bemis Street. No building shall exceed the height of the courthouse, 81 feet.

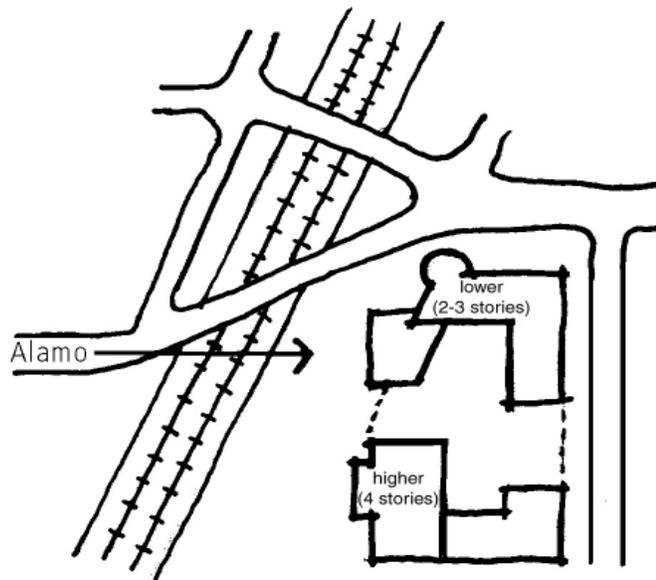


2C1.2: The building (s) along Bemis Street should respect the scale and pattern of the houses across the street through such design strategies as the articulation of building form, column/pier spacing, window pattern, and similar approaches.



2C2: ORIENTATION TO STREET VIEWS, NEIGHBORHOOD, OPEN SPACE, DOWNTOWN, AND LIGHT RAIL STOP.

Goal: *To create interesting architectural responses at the terminus of the Alamo Avenue view, and at the corner of Littleton Boulevard and the railroad line.*



2C3: ARCHITECTURAL CHARACTER

Goal 1: *To treat all facades with exterior exposures as 'front' facades worthy of careful architectural treatment with quality materials.*

Goal 2: *To insure that the higher more prominent building forms receive exceptional architectural treatment, particularly their roof treatment since they will be seen at some distance.*

2C4: MATERIALS AND COLORS

Goal: *To relate colors, and, as much as possible, materials to those used for the historic Arapahoe County Courthouse*

2C4.1: *As this is a 'Gateway' site, materials should particularly express quality and permanence.*

DESIGN GUIDELINES UNIQUE TO THE COMMUNITY CENTER SITE

3A: SITE PLAN/URBAN DESIGN

No additional goals or guidelines for this site – see design guidelines common to both the Sheriff's Department and the Community Center Sites.

3B: LANDSCAPE ARCHITECTURE

No additional goals or guidelines for this site – see design guidelines common to both the Sheriff's Department and the Community Center Sites.

3C: ARCHITECTURE

3C1: GROUND FLOOR USES

Goal: *To encourage some active ground floor retail uses along Littleton Boulevard.*

3C1.1: Buildings should orient facades and entries toward the street, or toward a plaza or pedestrian way that directly leads to a street.

3C1.2: Each multi-story building should have one clearly identifiable “front door” that addresses Littleton Boulevard.

3C2: BUILDING TRANSPARENCY

Goal: *To provide transparent glazing at the ground floor that insures the visibility of active uses, and goods.*

3C2.1: Where functionally appropriate, the majority of the ground floor, street-facing façade shall be made of transparent materials designed to allow pedestrians to view activities inside the buildings, or displays related to these activities.

3C3: FAÇADE QUALITY

Goal: *To design primary building facades that face residential streets with the highest quality, character, and sale.*

3C3.1: Provide for human scaling design elements for the facades that connect the residential streets.

LITTLETON BOULEVARD CORRIDOR STUDY

Vision

Littleton Boulevard will become a great street with a mix of commercial and residential uses and an active and comfortable pedestrian environment. The street will provide excellent transit, pedestrian, and auto connections to downtown, the light rail station, and the surrounding neighborhoods.

Goals

- Establish Littleton Boulevard as one of the great streets in the metro area.
- Identify and strengthen the character and community role of Littleton Boulevard.

Objectives

- Identify potential uses that support the role of the street and compliment downtown and the Broadway corridor uses.
- Identify and strengthen the role of the street as a multi-modal connection within the City.
- Identify catalyst sites for redevelopment.
- Develop a street that fits the City of Littleton image and character.
- Encourage zoning and land use policies that promote a mix of uses along the corridor.
- Provide zoning recommendations and urban design policies that reflect market forces and financial constraints.
- Identify focal points along the corridor that support connections to adjacent neighborhoods and parks.
- Identify gateway opportunities into and from downtown Littleton.
- Preserve and strengthen the identity of the adjacent historic residential neighborhoods.
- Improve pedestrian and bicycle circulation and safety.

Implementation

- Establish a new 'overlay' zone for the Littleton Boulevard corridor.**
In order to achieve the planning and design objectives envisioned in this study, it will be necessary to guide new development and significant renovations in the direction of these objectives. Regulations are the most effective and comprehensive method of directing redevelopment, other than the City playing a financial role in the redevelopment. Current zoning regulations, while able to accommodate most of the desired development, are not tailored to specifically encourage the type of development described in the study, while discouraging other forms of development that do not meet the study's objectives. Rather than a piecemeal rezoning strategy, where each and every rezoning becomes an individual battle, it would be far more preferable to establish a new 'overlay' zone for the Littleton Boulevard corridor, which would comprehensively address the necessary regulatory requirements. These requirements would probably include the following categories: 1) a design review process; 2) design review standards and guidelines; 3) and potentially some density, height, setback, and use changes. The overlay zone would be drawn in such a way to focus the regulatory changes only onto the area outlined in the study.

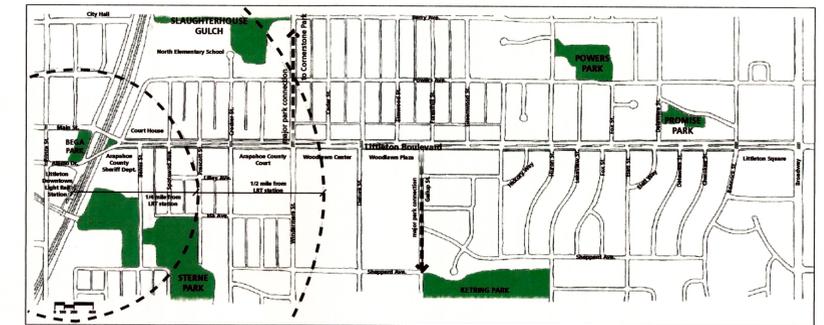
Review by the Historic Preservation Board should also occur at this time to ensure that the proposed architecture is compatible for the corridor. The district should allow both higher density multi-family and supportive commercial developments compatible with higher volume commercial corridors and transit oriented development.
- Work with RTD to develop higher amenity transit stations along the corridor.**
The Littleton corridor is being promoted as a mixed-use residential street with strong pedestrian and transit connections to the light rail station. The transit stations should therefore be distinctive and of a higher quality as part of the streetscape amenity package. RTD provides some funding for small community transit improvements, often in the form of matching funds. The City should work with RTD to plan and fund higher quality bus transit stations along the corridor.
- Develop the detention area south of the Sheriff's Facility to a City Park.**
The detention area south of the Sheriff's Facility currently serves as a regional detention facility and controls water entering the flume before it enters the Little's Creek drainage. This area should become a useable City park through the introduction of pedestrian amenities, landscape materials, and adequate irrigation. The park should allow for future trail connections along the City ditch easement and a bridge connection to the Littleton Downtown Light Rail Station and the Little's Creek Trail.
- Improve Pedestrian Connections**
The City of Littleton should provide improved pedestrian connections from Littleton Boulevard to Sterne Park along Bemis Street, to Kettering Park along Gallup Street, and to Powers Park along Delevare Street. Improvements should include pedestrian scaled lighting, street trees where possible, and pedestrian amenities such as; seating, signage and way-finding, and improved walk surfaces where needed.



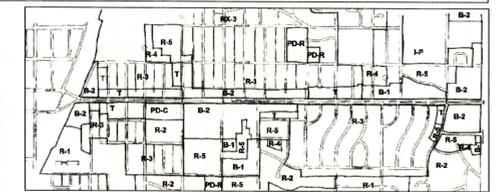
Proposed Littleton Boulevard Character Sketch



Existing Littleton Boulevard



Connections Map



Existing Zoning

Recommendations

Market

- Redevelop both the north and south sides of the street between Windermere and Hickory with medium density residential buildings. Three and four-story buildings at densities up to 50 units per acre are supportable.
- Develop first level retail and commercial uses where appropriate to enliven the ground floor of buildings fronting on the street.
- Plan for the future development of a neighborhood shopping center or mixed-use village center on the Woodlawn Center property. A neighborhood center could be anchored by a supermarket of 55,000 SF and contain a total of 80,000 to 100,000 SF. A village center development including 25,000 to 30,000 SF of neighborhood commercial uses and 200 to 300 residential units at mixed densities.
- Marketable uses do not support acquisition and redevelopment of existing uses at current land prices. Land values will appreciate, but will take 5 to 10 years to support redevelopment.
- More immediate redevelopment opportunities are limited to vacant sites, sites that can be assembled or redeveloped using alternative revenue sources, have redevelopment potential due to public use changes, or public land sales.

Circulation

- Maintain on-street parking on the north-south streets that intersect Littleton Boulevard.
- Provide more frequent bus service and connections to Downtown Littleton and the Light Rail Station.
- Improve pedestrian linkages between Littleton Boulevard and Kettering Park, Stearns Park, and Powers Park.
- Develop clearly marked pedestrian crossings with pedestrian walk signals.
- Develop attractive and generous pedestrian and bike facilities to reinforce transit service and humanize the street.
- Reduce the number and size of curb cuts.
- Continue to study the possibility of a pedestrian connection across the railroad depression at the Littleton Downtown Light Rail Station to better connect the Littleton Boulevard Corridor area with downtown.

Character and Image

- Redevelop Littleton Boulevard as a mixed-use street with predominantly residential uses.
- Locate parking at the rear or sides of new development.
- Provide detached sidewalks with street trees and pedestrian lights on both sides of the street.
- Develop identifiable entries at the downtown and Broadway ends of the street.
- Develop a distinctive signage and wayfinding system that is coordinated with signage for downtown and with historic reference signs throughout the area.
- Provide consistent and higher quality architecture, landscaping, and continued streetscape improvements to create a street that has a desirable address.
- Promote the dedication of a new park south of the Sheriff's Facility at the existing detention area.

Development Catalyst Sites

1. Arapahoe County Sheriff's Facility

The Arapahoe County Sheriff's Facility at the corner of Littleton Boulevard and Bemis Street will soon be vacated. It is the closest redevelopment site to the Littleton Downtown Light Rail Station and therefore a key catalyst site for the corridor. Medium density residential uses are recommended. The detention pond south of this site should be made into a formal park with landscape and pedestrian improvements. The City should continue to pursue a pedestrian connection across the railroad depression connecting the Downtown Light Rail Station to the east side of the depression. The scale of any new development on this site should complement the scale of the Historic Courthouse on the north side of Littleton Boulevard.

This site is currently zoned B-1 which does not allow residential development and allows some commercial uses incompatible with the redevelopment plan. The County's interest will be to sell the property for its maximum value. If the site were rezoned for transit-oriented residential development at 40 units per acre, the property could have a value equal to or greater than its current B-1 zoning.

2. South Suburban Community Center

The Community Center site located between Spotswood Street and Prescott Street on the south side of Littleton Boulevard will be moving to a new building north of the historic Courthouse. This site will be sold by the South Suburban Parks and Recreation District. This location is a great opportunity for infill office, or residential uses because of its close proximity to the light rail station and Downtown Littleton.

3. Woodlawn Center

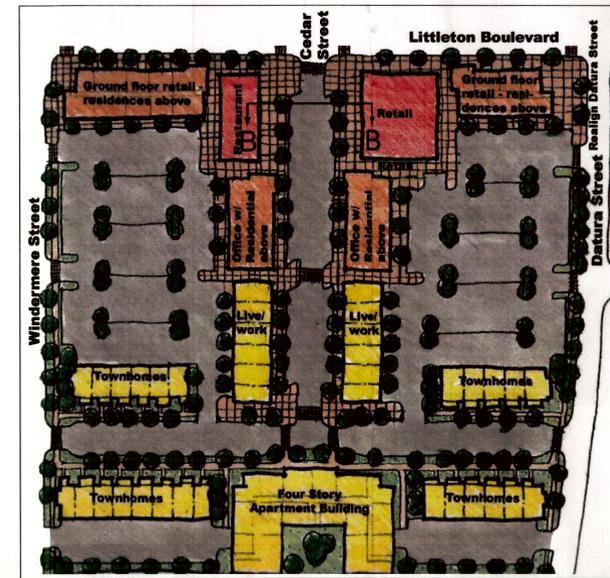
Woodlawn Center, at the southeast corner of Littleton Boulevard and Windermere Street, is a prime site for redevelopment due to its larger size (9 acres), age, and existing uses. A mix of uses including residential (both rental and ownership), retail, office and live/work are recommended. Uses should front onto Littleton Boulevard and onto the side streets of Windermere and Datura. On-street parking should be provided on Windermere and Datura Streets, and on internal circulation streets.

4. Woodlawn Plaza

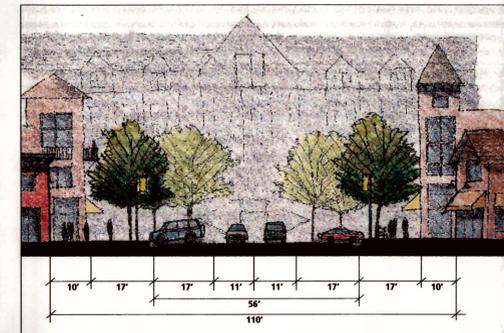
Woodlawn Plaza, east of Woodlawn Center between Datura Street and Gallup Street should provide uses that help support the redevelopment of the street. Because of the site size and the goals of the corridor, the predominant use should be three to four story residential with retail and commercial uses on the ground floor facing Littleton Boulevard. Adequate connections from parking behind the retail uses should be provided.



Plan of Possible Residential Redevelopment on Sheriff Department Site



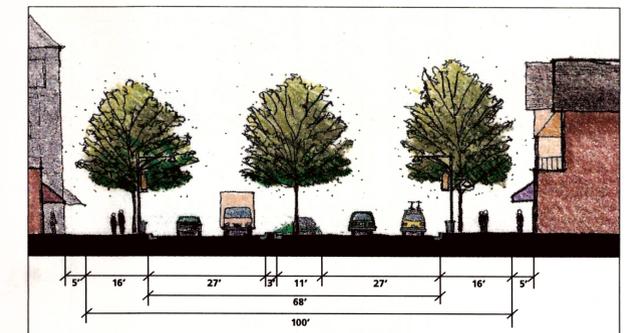
Plan of possible Woodlawn Center Redevelopment



Proposed Section B-B: Woodlawn Center Redevelopment (looking South from Cedar Ave)



Existing Woodlawn Center



Proposed Section A-A: Littleton Boulevard Between Windermere and Datura Streets



Existing Littleton Boulevard between Windermere and Datura Streets

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